

Application Recommended for Delegation

APP/2009/0695

Hapton with Park Ward

Full Planning Application

Proposed installation of shop front, erection of rear porch, external alterations, creation of five apartments, access alterations and laying out of parking.

BRIDGE HOUSE INN, 4 MANCHESTER ROAD, HAPTON

Background:

The application relates to a vacant public house. An objection has been received.

Planning permission was granted in 2008 for conversion of the whole building to 9 apartments. The scheme included layout of the rear yard as a residents' car park with 12 spaces and access from Water Street.

Use of the ground floor and basement as a shop does not need planning permission as that is granted as 'permitted development' by the Town and Country Planning (General Permitted Development) (Amendment) Order 2005

Summary of Reason for Recommendation:

The development is generally in accordance with the Development Plan, in particular the policies listed below, and there are no other material considerations to indicate that planning permission should not be granted:

Relevant Policies:

Burnley Local Plan Second Review

E15 - Locally important buildings, features and artefacts

GP1 - Development within the Urban Boundary

GP3 - Design and Quality

H15 - Conversion and re-use for flats and bedsits

H2 - The sequential release of further housing land for development

TM15 - Car parking standards

CF12 – Local and Village Shops

Site History:

2008/0186: Conversion of existing public house into 7 apartments - Granted

APP/2008/0372: Change of use of former public house to 9 no apartments re-submission of application (APP/2008/0186) - Granted

Consultation Responses:

Neighbouring Resident (Water Street) – Letter making objection on the following grounds:

1. Increased traffic in Water Street, already difficult.
2. 11 parking spaces would be insufficient for 5 flats plus retail, therefore there would be overspill parking on Water Street.
3. Lack of outside space for the development.

British Waterways – Recently consulted on the application. No objection to the previous proposal. I will report any comments at the meeting.

Highway Authority (Lancashire County Council) – Recently consulted. Initial response indicates no objection as regards arrangements for residents’ parking. Noted that the shop use is permitted development, but in any event street parking restrictions (double yellow lines) are in place.

Health and Safety Executive – Standard advice, in relation to adjacent chemical industry, does not advise against the development.

Planning and Environmental Considerations:

The application relates to a traditional canalside public house, fronting Manchester Road, in the centre of Hapton Village. It is of stone and slate construction being mainly two-storey, with a partial lower ground floor on the Canal frontage. It has a rear yard with access from Water Street. The building dates from the latter part of the nineteenth century and is of considerable character. It is now vacant.



Front elevation

Previously granted (2008)

Residential conversion comprising:

- *One (2 bedroom) apartment on the lower ground floor;*
- *Three (2 bedroom) apartments on the ground floor;*
- *Three (2 bedroom) apartments on the first floor; and,*
- *Two (1 bedroom) apartments in the converted roof space.*

Access to all the 9 units would be via the existing two front street doors (one being reopened) and newly constructed internal stairways.

The scheme included parking for 12 cars in the rear yard, with access from Water Street.

The existing roof space would be increased in usable floor area by alteration to the roof of the building. This would involve provision of one pitched roof over the whole building, replacing the existing arrangement comprising two separate pitched roofs, joined in a valley, difficult to maintain, and in poor condition. The ridge line of the new roof would be 1.1m higher and 1.5m to the rear of the line of the existing ridge. The chimneys stacks would be retained.



Rear of building; yard and access from Water Street

The present proposal comprises:

- Three (1- bed) apartments on the first floor; and,
- Two (1-bed; 2-bed) apartments in the converted roof space.

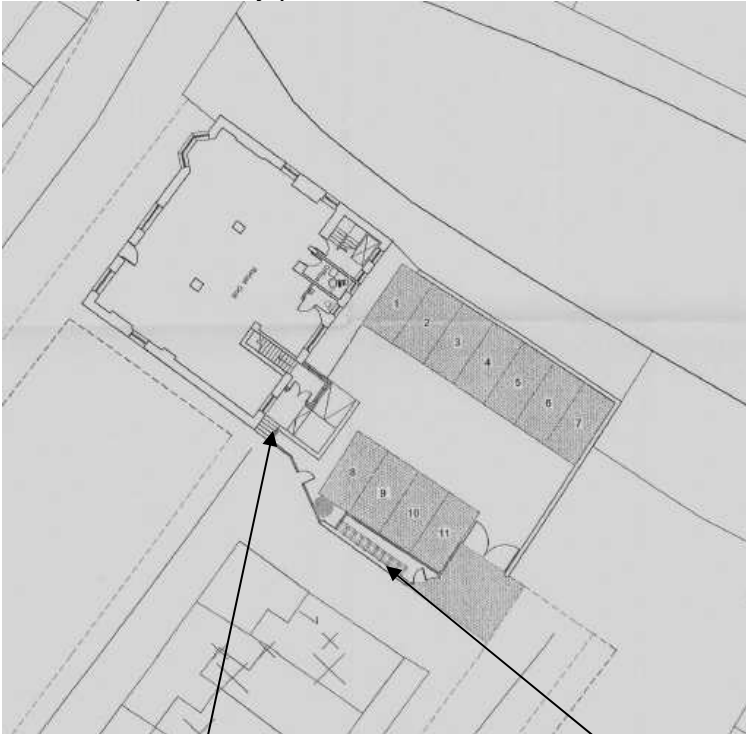
Access to all the 5 apartments would be from the rear a newly constructed entrance porch and a new internal stairway.

Parking for 11 cars would be provided in the rear yard, with a gated access to/from Water Street.

The alterations to the roof would be as previously granted, however the two chimney stacks would not be retained.

Matching natural stonework would be used in the minor alteration (to window and door openings; and, removal of rear external stone stairs and low wall). The Canal side gable wall is of stonework; the other gable is rendered. The gables would be built up in

matching materials. There would be one additional small upper floor window in each gable compared with the previously permitted scheme.



Layout plan

Rear porch access to apartments - Bin store inside retained boundary wall



Proposed front and rear elevations

This application should be assessed in terms of any additional impact of the present proposal compared with the 'fall-back' development previously granted.

The main planning considerations are, traffic and parking, design, including roof alterations, and shop front design and appearance.

Traffic and parking

The relevant criteria set by **Policy H15** are:

- a) *No adverse effect on amenity of neighbouring residents*
- b) *Convenient access (Car parking to comply with car parking standards in Policy TM15)*

Policy TM15 sets out parking standards required. 12 parking spaces were provided in the previous scheme for 9 apartments; on that basis the 11 spaces provided in the present scheme would be more than adequate for residents of the 5 apartments proposed.

The yard was previously available for parking in connection with the whole building including the ground floor drinking establishment. On that basis I consider it reasonable and necessary to impose a planning condition requiring that one of the parking space be kept and marked out for use by shop staff during opening hours. I take a different view on customer car parking. I do not consider it desirable to encourage movement of customers into the residential streets at the rear of the premises as it would be likely to harm residential amenity from the coming and going of vehicles. Therefore I would not recommend a requirement to provide an element of customer parking in the rear yard.

The Highway Authority has no objections to the proposal on safety grounds, and considers the parking adequate to meet the new residents' requirements. Hapton village has a railway station within walking distance, and main-line bus services to Padiham and Burnley. The opportunity to use the Canal cycle route should be reinforced by provision of secure cycle stores and a condition is recommended to require provision to be made.

Residents of the proposed apartments would access the car park via Water Street, which has a residential terrace on one side and bungalows on the other. However, the amenity of neighbouring residents is unlikely to be affected more than if the Public House was fully active, or than in the previously granted schemes, which are the various fall-back positions available to the applicant in this respect.

Design and impact of the new roof

Policy H15 requires that the converted building be *sympathetic to the character and appearance of the building and its locality.*

The character and future of the building would be safeguarded by the proposal. The building is well suited to conversion to apartments and the floor plans show a good standard of accommodation. Good pedestrian access would be provided. Refuse/recycling bin storage is shown in purpose-designed units in the rear yard area.

The roof ridge would be raised by a modest amount, given the significant expanse of roof involved. Dwellings in Manchester Road face the front of the Inn – the raised element of roof would be 18m from habitable room windows, which would retain their outlook in accordance with Local Plan policy.

The roof shape of the building would change; the street scene would be different, but not harmed. I take a balanced view on the omission of the 3 chimney stacks from the present scheme. Whilst keeping traditional features of the building is desirable, their retention would have an awkward appearance as they would be part-way down the front roof slope. Therefore I feel that removal of the chimneys, which would enable a more straightforward roof design and weatherproof covering, would be equally acceptable.

A condition is recommended to ensure matching natural stone, rendering and natural roof slate, or a suitable equivalent, are used in the development.

Shop front design

My opinion is that as far as possible the proposed shop front should work together with existing features of the building to help retain its character and appearance.

For this reason I am seeking further details and clarification about the treatment of the shop front and will present these at the meeting.

Recommendation: That provided satisfactory details of the treatment of the proposed shop front are received the Head of Planning and Environment be delegated to grant permission subject to the following conditions:

1. The development must be begun within three years of the date of this decision.
2. Notwithstanding any details specified in the application:
 - (a) Any alterations or building up of the masonry walls of the building shall be finished externally in natural stone matching the existing stonework of the building in colour, texture, coursing and jointing unless other materials are approved in writing by the Local Planning Authority.
 - (b) Any alterations or building up of rendered walls shall be finished externally in render matching the existing rendering unless other material are approved in writing by the Local Planning Authority.
 - (c) Any alterations or building up of the roof shall be finished externally in natural slate unless other materials are approved in writing by the Local Planning Authority.
3. Unless otherwise approved in writing by the Local Planning Authority, any alterations to or provision of new window or door openings in the walls of the building shall be as shown on the submitted drawing identified as: Project No KIND/36 Dwg 02F Dated 05.06.09; and, heads, cills and jambs (as shown) of those openings shall be of natural stone matching the existing stonework of the building in colour, texture, size and jointing.

4. Before any development starts, details of provision for the secure, covered storage of bicycles, shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented before the building hereby approved is first brought into use and thereafter be maintained and kept available for the use of occupiers of the premises.
5. Before any development starts, a scheme for the treatment of the land within the site to the east of the building as a car park shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall show details of (a) the surface treatment of the land; (b) the means of enclosure of the land; and, (c) the layout and marking out of car parking spaces. The approved scheme shall be carried out before the residential apartments hereby permitted are first occupied and the car parking spaces shall be retained thereafter for parking of occupiers of the building (including one space kept available for use by staff of the shop during the opening hours thereof).
6. Vehicular access to the car park referred to in condition 5 shall be as shown on the submitted drawing identified as: Project No KIND/36 Dwg 05A Dated 07.03.08. There shall be no vehicular access to the street between the car park and the gable wall of No1 Water Street, or to the street at the back of that property, at any time.
7. Before any apartment is first occupied the existing cellar access hatch set in the footway along the Manchester Road frontage of the building shall be removed and the opening made good in design and materials matching and forming part of the existing footway.

Reasons:

1. Required to be imposed by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.
2. To secure a satisfactory development in materials which are appropriate to the building and locality, in the interests of visual amenity, in accordance with Policy H15 of the Burnley Local Plan Second Review.
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4. The provision is required to promote alternative forms of transport to the private car and details are not provided in the application, in accordance with Policy TM15 of the Burnley Local Plan Second Review.
5. In the interests of visual amenity, to minimise noise and disturbance to neighbouring residents from traffic to the site, and to ensure adequate off-street car parking is provided and retained, in accordance with Policies H15 and TM15 of the Burnley Local Plan Second Review.

6. In the interests of visual amenity, to minimise noise and disturbance to neighbouring residents from traffic to the site, and to ensure adequate off-street car parking is provided and retained, in accordance with Policies H15 and TM15 of the Burnley Local Plan Second Review.
7. In the interests of highway safety.

AR
16.12.2009